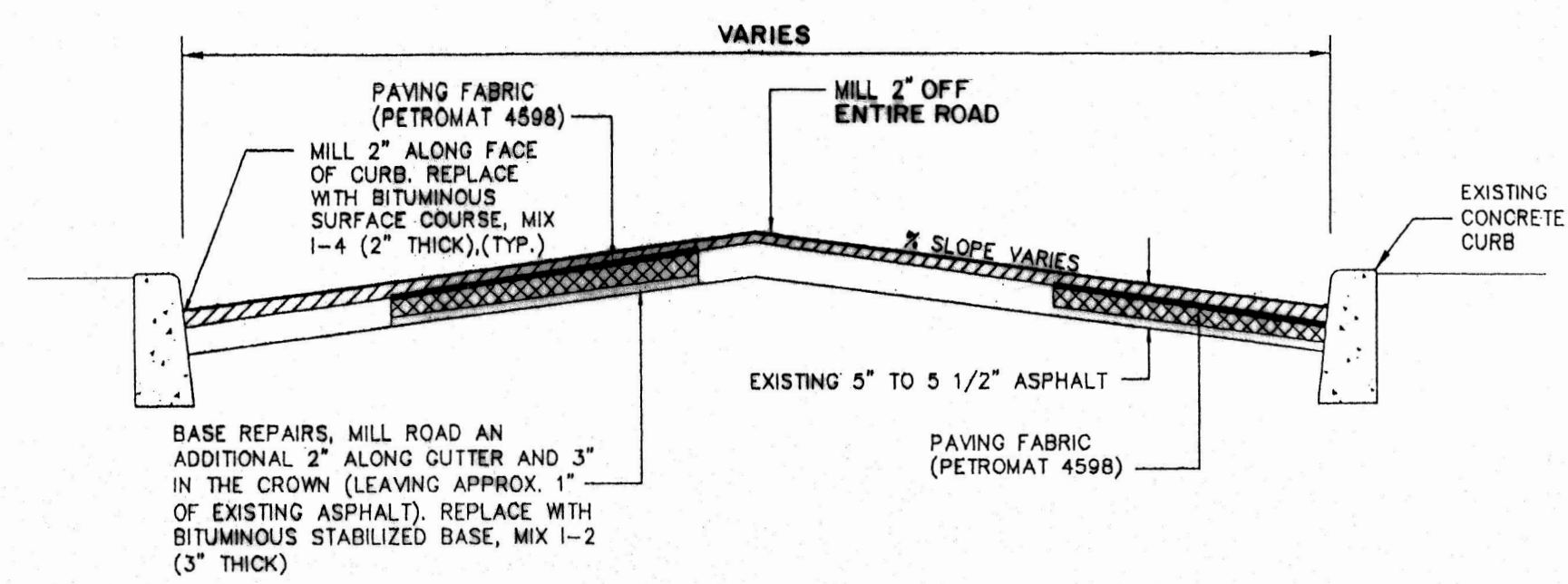
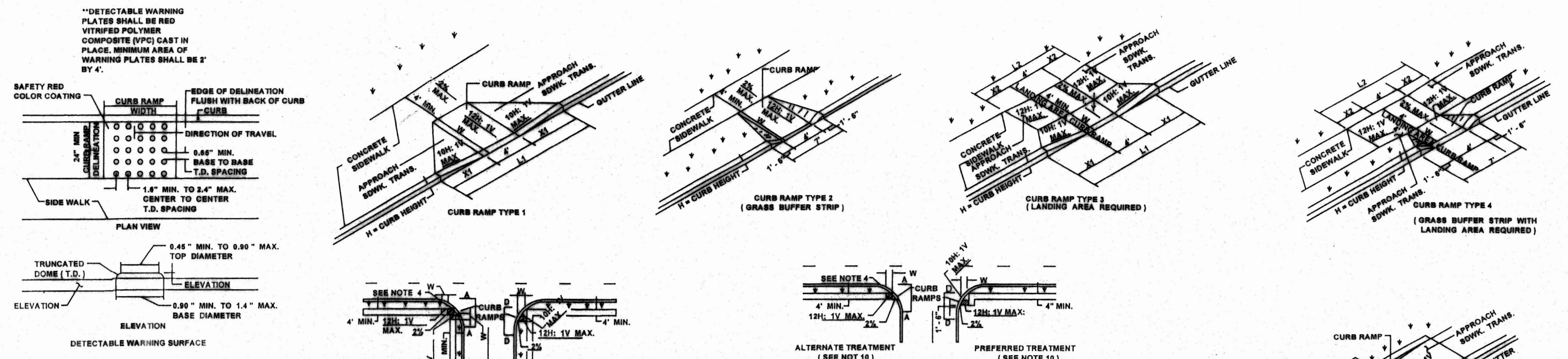


INLET REHABILITATION DETAIL
N.T.S.

- NOTES:**
- TYPE "B" INLET FRAME AND GRATE TO BE CAMPBELL FOUNDRY CASTING NUMBER 2618 TYPE "J" BICYCLE SAFE GRATE OR APPROVED EQUAL.
 - TYPE "E" INLET FRAME AND GRATE TO BE CAMPBELL FOUNDRY CASTING NUMBER 3425 BICYCLE SAFE GRATE OR APPROVED EQUAL.
 - TYPE "A" INLET FRAME AND GRATE TO BE CAMPBELL FOUNDRY CASTING NUMBER 3405 BICYCLE SAFE GRATE OR APPROVED EQUAL.
 - ALL GRATES AND HEADERS SHALL BE PHASE II STORM WATER COMPLIANT WITH WORDS "DUMP NO WASTE DRAINS TO WATERWAYS" AND FISH SYMBOL CAST ON THEM.
 - "B" INLET HEADERS SHALL BE 8" HIGH AND 8" WIDE ON TOP TO MATCH PROPOSED CURB.
 - CONTRACTOR IS RESPONSIBLE TO ORDER CORRECT SIZE GRATES AND HEADERS AS SPECIFIED TO MATCH EXISTING CURBS AND HEADERS BETWEEN EXISTING CURBS TO REMAIN.
- GENERAL:**
- THE CONTRACTOR IS RESPONSIBLE TO REMOVE DEBRIS FROM ALL INLETS LOCATED WITHIN THE SCOPE OF WORK.
 - INLET REHABILITATION SHALL INCLUDE REMOVING THE EXISTING CASTINGS; REPAIR EXISTING WALLS, LADDER RUNGS AND REPAIR OR INSTALL A NEW LOW FLOW CHANNEL. IN ADDITION, THE INSIDE OF THE INLET SHALL BE FARGED WITH A 1/2" THICK COAT OF APPROVED WATERPROOF CEMENT TROWELED TO A SMOOTH FINISH AND THE NEW CASTING SET TO GRADE WITH WATERPROOF CEMENT TROWELED AROUND THE OUTSIDE OF THE INLET FRAME. BACKFILLING SHALL BE DONE WITH SUITABLE MATERIAL IN LIFTS OF SIX (6) INCHES OR LESS AND COMPACTED AS SPECIFIED.
 - PAVEMENT TO BE MILLED AND REPAVED SURROUNDING THE EXISTING INLET SHALL BE PAVED SUCH THAT TO ALLOW POSITIVE DRAINAGE INTO THE CATCH BASIN. UNLESS NOTED IN THESE PLANS, THE CONTRACTOR SHALL NOT ALTER THE ELEVATION OF THE NEW FRAME AND GRATES FROM EXISTING CONDITIONS UNLESS APPROVED BY THE TOWNSHIP DPW OR THE ENGINEER.



TYPICAL ROADWAY & MILLING SECTION
N.T.S.



GENERAL NOTES:

- WHERE PRACTICAL END LEFT TURN ISLAND LANDING AREA, APPROACH SIDEWALK TRANSITIONS, AND CURB RAMP SHALL BE KEPT CLEAR OF OBSTRUCTIONS.
- DIMENSIONS SHOWN IN TABLES ARE FOR RELATIVELY FLAT SIDEWALK AREAS. CARE SHOULD BE TAKEN WHEN DETERMINING CURB RAMP SIZE BASED ON CURB HEIGHT (H) WHEN ELEVATION OF CURB AND SIDEWALK VARY DRASTICALLY IN AREA OF PROPOSED CURB RAMP.
- CURB (DROPPED CURB) GUTTERLINE TO BE FLUSH WITH ROADWAY PAVEMENT A MINIMUM OF 4 FEET AT ALL CURB RAMP.
- FOR CURB RAMP TYPES 5 AND 6, IF A GRASS BUFFER DOES NOT EXIST, SLOPE CURB TO EQUAL SLOPE OF ADJACENT CURB RAMP.
- SIDEWALK AND CURB RAMP WITHIN AREA ENCLOSED BY HEAVY LINES TO BE PAID FOR AS CONCRETE SIDEWALK OF THE APPROPRIATE ADJACENT THICKNESS.
- CURB AND HEADER WITHIN AREA ENCLOSED BY HEAVY LINES TO BE PAID FOR AS VERTICAL CURB OR SLOPE CURB OF THE APPROPRIATE ADJACENT SIZE AND RISE.
- WHERE THE DISTANCE FROM GUTTER LINE TO THE OUTSIDE EDGE OF SIDEWALK IS 8 FEET OR LESS, CURB RAMP TYPE 7 SHOULD BE USED, INSTEAD OF CURB RAMP TYPE 1 THROUGH 4.
- THE PUBLIC SIDEWALK CURB RAMP, DETECTABLE WARNING SURFACE (SHADDED AREA) SHALL BE RED.
- CROSSWALKS AND STOP LINES MAY BE MARKED OR UNMARKED, SEE PLANS.
- PREFERRED AND ALTERNATE TREATMENTS SHOULD NOT BE INTERMIXED WITHIN THE SAME INTERSECTION.
- DIMENSIONS SHOWN IN TABLES ARE FOR 3 INCHES CURB HEIGHTS, WHERE THE CURB HEIGHTS ARE OTHER THAN WHAT IS PROVIDED IN THE TABLES, THE DIMENSIONS OF THE RAMP WILL HAVE TO BE CALCULATED BASED ON CROSS SLOPES SHOWN.

TABLES:

W	H	L1	L2	L3
2.0	2.0	1.0	1.0	1.0
3.0	3.0	1.5	1.5	1.5
4.0	4.0	2.0	2.0	2.0

W	H	L1	L2	L3
2.0	2.0	1.0	1.0	1.0
3.0	3.0	1.5	1.5	1.5
4.0	4.0	2.0	2.0	2.0

W	H	L1	L2	L3
2.0	2.0	1.0	1.0	1.0
3.0	3.0	1.5	1.5	1.5
4.0	4.0	2.0	2.0	2.0

W	H	L1	L2	L3
2.0	2.0	1.0	1.0	1.0
3.0	3.0	1.5	1.5	1.5
4.0	4.0	2.0	2.0	2.0

SECTION A-A
NOTE: CURB RAMP OPENING TO BE FLUSH WITH ROADWAY PAVEMENT (CURB RAMP TYPES 5 & 6).
VARIABLE SIDEWALK WIDTH
12" MIN. TO 24" MAX.

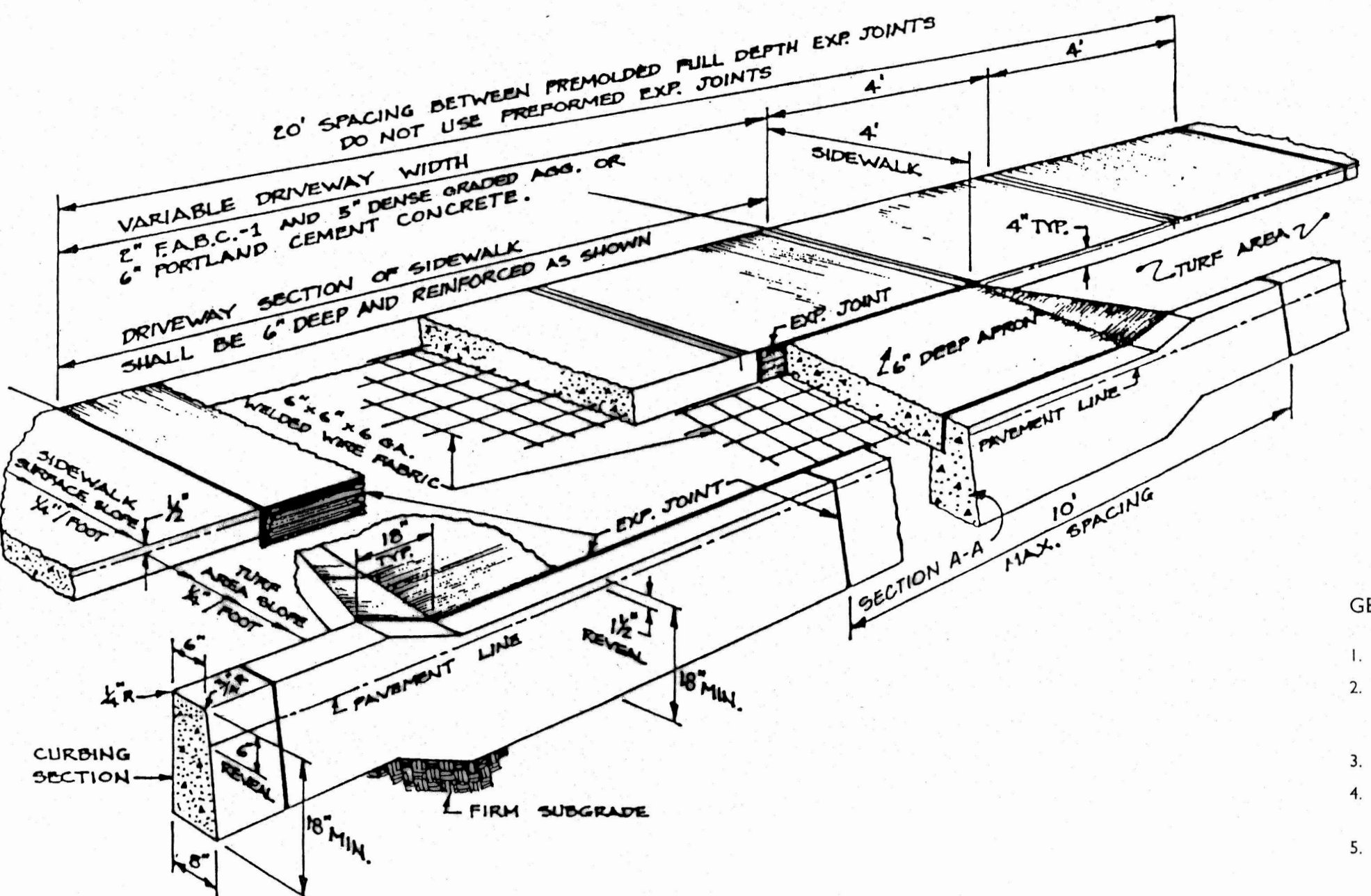
SECTION D-D
NOTE: CURB TO BE FLUSH WITH ROADWAY PAVEMENT.
VARIABLE CURB RAMP WIDTH
12" MIN. TO 24" MAX.

SECTION THROUGH CURB RAMP 1 THROUGH 6
NOTE: CURB RAMP OPENING TO BE FLUSH WITH ROADWAY PAVEMENT.
VARIABLE CURB RAMP WIDTH
12" MIN. TO 24" MAX.

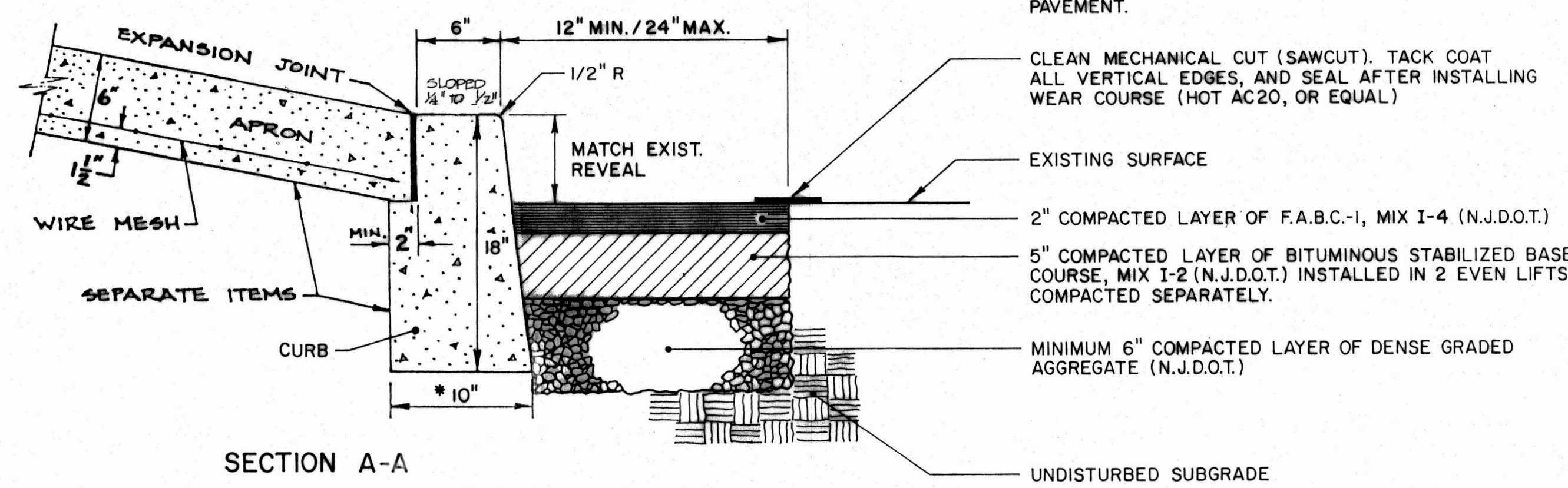
HANDICAP ACCESSIBLE RAMP DETAIL
N.T.S.

- GENERAL NOTES:**
- THE LOCATION OF EXISTING UTILITIES SHOWN ON THIS PLAN HAS BEEN TAKEN FROM VARIOUS SOURCES AND IS SHOWN FOR DESIGN AND ESTIMATING PURPOSES ONLY. ACTUAL LOCATIONS OF UTILITIES SHALL BE FIELD VERIFIED. ANY DISCREPANCIES OR APPARENT CONFLICTS SHALL BE CALLED TO THE ATTENTION OF THE ENGINEER FOR INTERPRETATION. CONTRACTORS ARE ADVISED TO CALL 1-800-272-1000 BEFORE COMMENCING CONSTRUCTION FOR A MARKOUT OF EXISTING UTILITIES.
 - EXISTING UTILITIES INCLUDING VALVE BOXES, CLEAN OUTS, CURB STOPS, MANHOLES, METERS, ETC. SHALL BE ADJUSTED TO MATCH PROPOSED FINISHED GRADE. STORM DRAINAGE STRUCTURES SHALL NOT BE ADJUSTED UNLESS SPECIFICALLY CALLED FOR.
 - WHERE PROPOSED PAVEMENT ABUTS EXISTING PAVEMENT, THE EXISTING PAVEMENT SHALL BE SAW-CUT TO PROVIDE A SMOOTH VERTICAL EDGE. ALL ASPHALT JOINTS SHALL BE SEALED WITH A HOT Poured ASPHALT JOINT SEALER CONFORMING TO THE REQUIREMENTS OF ASTM D1190. ALL EXISTING JOINTS SHALL BE SEALED PRIOR TO PLACEMENT OF THE FABC OVERLAY.
 - THE CONTRACTOR SHALL PROPERLY DISPOSE OF ALL SPOILS AND EXCAVATED MATERIALS.
 - INSPECTION OF, OR FAILURE TO INSPECT ANY MATERIALS OR WORKMANSHIP BY THE ENGINEER SHALL IN NO WAY RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY TO PERFORM THE WORK IN ACCORDANCE WITH THE PLANS, SPECIFICATIONS AND APPLICABLE LAWS.
 - MEET EXISTING GRADES AT ALL LIMITS OF CONSTRUCTION.
 - PAVEMENT MARKINGS AND SIGNS ARE TO CONFORM WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - CONTRACTOR SHALL MAINTAIN TRAFFIC DURING CONSTRUCTION.
 - CONTRACTOR TO INSTALL AND MAINTAIN ALL SOIL EROSION CONTROL MEASURES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN LUMP SUM AMOUNT FOR ITEM TURF REPAIR. THE ITEMS SHALL INCLUDE SILT FENCE, INLET PROTECTION, ETC.
 - ALL EXISTING MUA MANHOLE FRAMES AND COVERS SHALL MATCH FINAL PAVEMENT GRADE. IF NECESSARY, FRAMES AND COVERS SHALL BE ADJUSTED TO GRADE WITH CONCRETE RINGS AND NON-SHRINKING GROUT IN CONFORMANCE WITH EVMUA RULES AND REGULATIONS.
 - ALL BACKFILL AND BITUMINOUS MATERIALS SHALL BE INSTALLED IN A MANNER CONSISTENT WITH TOWNSHIP ORDINANCE NOS. 15-13 AND 15-110 RULES AND REGULATIONS FOR EXCAVATIONS AND SURFACE RESTORATION. CONTRACTOR SHALL ALSO PROVIDE THE APPROPRIATE PROCEDURE TO ESTABLISH AND CERTIFY A MIN. OF 95% MOD. PROCTOR DENSITY OF ALL COMPACTED BACKFILL MATERIAL, SUBJECT TO TOWNSHIP APPROVAL.

- GENERAL NOTES:**
- ALL CONCRETE SHALL BE 4000 P.S.I.
 - ALL EXPANSION JOINTS SHALL BE 1/2" PRE-MOLDED BITUMINOUS AND FULL DEPTH FOR EACH ITEM OF CONCRETE (PREFORMED EXPANSION MATERIAL IS UNACCEPTABLE AND SHALL NOT BE USED).
 - WIRE MESH SHALL BE 6" X 6" X 6 GA. WELDED WIRE MESH.
 - ALL CONCRETE ITEMS SHALL BE PLACED ON A FIRM SUBGRADE. ANY UNSUITABLE CONDITION SHALL BE CORRECTED PRIOR TO PLACEMENT OF CONCRETE.
 - ALL CONSTRUCTION JOINTS SHALL BE HALF-DEPTH AND SHALL BE CONSTRUCTED USING A 2" X 2" ANGLE IRON OR OTHER METHOD APPROVED BY THE ENGINEER.
- NOTE:**
NO SEPARATE PAYMENT SHALL BE MADE FOR PAVEMENT RESTORATION ASSOCIATED WITH CONCRETE CURB CONSTRUCTION OR REPLACEMENT. THE COST FOR THIS WORK SHALL BE INCLUDED AS SPECIFIED IN PAY ITEMS: CONCRETE CURB, REMOVE AND REPLACE OR NEW CONCRETE CURB IN THE BID PROPOSAL.



CONCRETE SIDEWALK & DRIVEWAY APRON DETAIL
N.T.S.



SECTION A-A

* 8" MIN. WIDTH TYPICAL WHERE THERE IS NO SIDEWALK, HANDICAP RAMP OR DRIVEWAY APRON ABUTTING CURB

NOTES & DETAILS

ROAD IMPROVEMENTS FOR EDWARD DRIVE, LYNNFIELD DRIVE, MAPLE STREAM ROAD & POPLAR RUN

DRAWN: MBL	EAST WINDSOR TOWNSHIP	DATE	NO.
APPROVED: RC	MERCER COUNTY, N.J.		
DATE: 4/12/23	16 LANNING BOULEVARD		
SCALE: AS NOTED	EAST WINDSOR, N.J.		
SHEET NO. 5 of 5			REVISIONS